

LEGAL DESCRIPTION

A PORTION OF LAND LYING WITHIN THE NORTHEAST QUARTER OF SECTION 12, TOWNSHIP 14 SOUTH, RANGE 66 WEST OF THE 6TH P.M. CITY OF COLORADO SPRINGS, COUNTY OF EL PASO, STATE OF COLORADO, DESCRIBE AS FOLLOWS: BASIS OF BEARINGS: BEARINGS ARE BASED UPON THE EASTERLY RIGHT-OF-WAY LINE OF SPACE CENTER DRIVE AS RECORDED UNDER RECEPTION NO. 2011/0950 OF THE EL PASO COUNTY RECORDS, MONUMENTED AT THE NORTHWEST CORNER OF LOT I, CREEKSIDE CENTER FILING NO. 3 AS RECORDED UNDER RECEPTION NO. 201712705 OF SIAD RECORDS WITH A BRAZE CAP IN ASPHALT STAMPED "PLS 32430" AND MONUMENTED AT THE SOUTHWEST CORNER OF LOT I CREEKSIDE APARTMENTS FILING NO. I AS RECORDED UNDER RECEPTION NO. 201170950 OF SAID RECORDS WITH A #5 REBAR, AND IS ASSUMED TO BEAR N 05°05'08" E, A FIELD MEASURED DISTANCE OF 648.52 FEET. BEGINNING AT THE NORTHWEST CORNER OF LOT I OF SAID CREEKSIDE CENTER FILING NO. 3; THENCE N 05°05'08" E ALONG THE EASTERLY RIGHT-OF-WAY LINE OF SAID SPACE CENTER DRIVE, A DISTANCE OF 648.52 FEET TO SOUTHWEST CORNER LOT LOF SAID CREEKSIDE APARTMENTS FILING NO. 1: THENCE S 89°59'13" E ALONG THE SOUTH LINE OF LOT 1. OF SAID CREEKSIDE APARTMENTS FILING NO. 1. A DISTANCE OF 375.04 FEET THE SOUTHEAST CORNER OF LOT 1. OF SAID CREEKSIDE APARTMENTS FILING NO. I SAID CORNER ALSO BEING A POINT OF THE WESTERLY RIGHT-OF-WAY LINE OF POWERS BOULEVARD AS RECORDED UNDER BOOK 5259 AT PAGE 1306; THENCE THE FOLLOWING THREE (3) COURSES ALONG SAID WESTERLY RIGHT-OF-WAY LINE OF POWERS BOULEVARD:

I) S 10°34'00" W, A DISTANCE OF 196.76 FEET 2) S 12°57'49" W, A DISTANCE OF 272.07 FEET

 $\frac{1}{1}$

3) 216.34 FEET ALONG THE ARC OF A 2587.00 FEET RADIUS NON-TANGENT CURVE TO THE LEFT, HAVING A CENTRAL ANGLE OF 04°47'29" AND A CHORD THAT BEARS S 06°58'28" W, 216,28 FEET TO THE NORTHEAST CORNER LOT I OF SAID CREEKSIDE CENTER FILING NO. 3; THENCE N 84°56'37" W ALONG THE NORTH LINE OF LOT 1 OF SAID CREEKSIDE CENTER FILING NO. 3 A DISTANCE OF 310.36 FEET TO THE POINT OF BEGINNING. CONTAINING A CALCULATED AREA OF 223,279 SQUARE FEET (5.126 ACRES) OF LAND, MORE OR LESS.

TO BE PLATTED AS CREEKSIDE PLAZA.

CONDITIONS OF RECORDS

ORDINANCE NO. 87-175 - PBC LOTS

- . THE FOLLOWING DESIGN PARAMETERS SHALL APPLY TO THE PROPERTY: A. SERVICE AREA SHALL BE LOCATED IN THE LEAST VISIBLE AREA OF THE SITE AND/OR BUILDING, AND SHALL BE APPROPRIATELY SCREENED WITH FENCING, EARTH BERMS, VEGETATION, OR COMBINATION THEREOF.
- B. HVAC SYSTEMS SHALL BE CONCEALED EITHER ARCHITECTURALLY, OR W APPROPRIATE TALL VEGETATION.
- C. LIGHTING SHALL BE OF A TYPE AND DESIGN SUCH THAT NO DIRECT GLARE IS VIEWED FROM THE ADJOINING STREETS AND PROPERTIES. D. THE EXTERIOR FEATURES OF ALL SIDES OF THE BUILDING SHALL
- SUBSTANTIALLY CONFORM TO THE STYLE OF THE FACADE. 2. THE 18.6 ACRE PORTION OF THE SITE SHALL BE RESTRICTED TO INDIVIDUAL PADS/FREESTANDING BUILDING.
- 3. DEVELOPMENT OF THE PROPERTY SHALL BE RESTRICTED TO THE TRAFFIC LEVELS/IMPACT AS SET FORTH ON PAGE 2 OF THE TRAFFIC IMPACT ANALYSIS PREPARED BY URS CORPORATION, DATED AUGUST 7, 1987 (EXHIBIT B). GREATER IMPACTS WILL BE ALLOWED IF A NEW STUDY INDICATES LEVELS OF SERVICE ACCEPTABLE TO THE TRAFFIC ENGINEERING DEPARTMENT.
- ORDI<u>nance no. 87 177 OC LOT</u> THE FOLLOWING DESIGN PARAMETERS SHALL APPLY TO THE PROPERTY A. SERVICE AREAS SHALL BE LOCATED IN THE LEAST VISIBLE AREA OF THE SITE \$/OR BUILDING, \$ SHALL BE APPROPRIATELY SCREENED WITH FENCING, EARTH BERMS, VEGETATION, OR COMBINATION THEREOF. B. HVAC SYSTEMS SHALL BE CONCEALED EITHER ARCHITECTURALLY,
- OR W/ APPROPRIATE TALL VEGETATION. C. LIGHTING SHALL BE OF A TYPE AND DESIGN SUCH THAT NO DIRECT GLARE IS VIEWED FROM THE ADJOINING STREETS AND PROPERTIES. D. THE EXTERIOR FEATURES OF ALL SIDES OF THE BUILDING SHALL
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GENERAL NOTES

- . COMMON ACCESS EASEMENT SHALL BE PROVIDED OVER THE LOTS TO BENEFIT. ALL OF THE LOTS WITHIN THE PBC ZONED AREA. . ALL STOP SIGNS" WILL BE INSTALLED BY THE DEVELOPER AT THE LOCATIONS SHOWN ON THE DEVELOPMENT PLAN TO MEET MUTCO STANDARDS. CONTACT TRAFFIC ENGINEERING, SIGNS & MARKING AT 719-385-6720 FOR ASSISTANCE.
- . PER THE COLORADO SPRINGS AIRPORT, AN AVIGATION EASEMENT SHAL BE GRANTED PRIOR TO RECORDING A FINAL PLAT. . A 25DB REDUCTION IN INTERIOR NOSE SHALL BE OBTAINED THROUGH SOUNDPROOFING USING FAA RECOMMENDED CONSTRUCTION TECHNIQUES, AND PERFORMED BY CERTIFIED ACOUSTICAL ENGINEER.
- THIS PROPERTY MAY BE IMPACTED BY NOISE CAUSED BY AIRCRAFT OPERATING INTO AND OUT OF THE COLORADO SPRINGS AIRPORT. THE BUYER SHOULD FAMILIARIZE HIMSELF/HERSELF WITH THIS POTENTIALITY. AND THE RAMIFICATIONS THEREOF
- . PROPERTY OWNERS ARE RESPONSIBLE FOR THE MAINTENANCE FOR SHARED COMMON TRACTS, ACCESS DRIVES, PRESERVATION AREAS AND LANDSCAPE AREAS ADJACENT TO THEIR PROPERTY
- REFERENCE GRADING PLAN (CP-2) FOR PROPOSED CONTOURS. . THE SAND CREEK TRAIL SHALL REPLACE THE DETACHED SIDEWALK ON THE WEST SIDE OF SPACE CENTER DRIVE, THEREBY ALLOWING A RIGHT-OF-WAY OF 60 FEET, INSTEAD OF THE NORMALLY REQUIRED 65
- . ALL IMPROVEMENTS DEPICTED WITHIN THIS PLAN ARE REQUIRED BY CITY CODE AND/OR ARE A CONDITION OF APPROVAL TO ENSURE COMPLIANCE WITH THE CITY'S REVIEW CRITERIA. ANY PRIVATE
- IMPROVEMENTS PROVIDED AS EXTRA, WILL BE LABELED AS "EX" AND WILL NOT BE INCLUDED IN THE CALCULATED AMOUNT REQUIRED FOR A FINANCIAL ASSURANCE. O. PRIOR TO CONSTRUCTION ACTIVITIES COMMENCING ON THE SITE, A GRADING AND EROSION CONTROL PLAN MUST BE APPROVED BY CITY
- ENGINEERING AND FINANCIAL ASSURANCES MUST BE POSTED FOR EROSION CONTROL AND PERMANENT WATER QUALITY FACILITIES. ALL CURB, DRIVEWAYS, PEDESTRIAN RAMPS AND SIDEWALK POSING A SAFETY HAZARD, DAMAGED, EXHIBITING EXCESSIVE DETERIORATION OR DOES NOT MEET CURRENT CITY ENGINEERING STANDARDS ALONG SPACE CENTER DRIVE ADJACENT TO THE SITE WILL NEED TO BE REMOVED AND REPLACED PRIOR TO THE ISSUANCE OF THE CERTIFICATE OF OCCUPANCY
- (CO). AN ON-SITE MEETING CAN BE SET UP WITH THE ENGINEERING DEVELOPMENT REVIEW DIVISION (EDRD) INSPECTOR TO DETERMINE WHAT IF ANY IMPROVEMENTS ARE REQUIRED. THE EDRD INSPECTOR CAN BE REACHED AT 719-385-5977
- 2. ALL THE PUBLIC SIDEWALK ADJACENT LOTS 1, 6, 7 AND 8 ALONG SPACE CENTER DRIVE SHALL BE INSTALLED PRIOR TO THE ISSUANCE OF THE FIRST CERTIFICATE OF OCCUPANCY (CO).
- 3. OWNER IS CREATING ASSOCIATION FOR THIS DEVELOPMENT TO INCLUDE MAINTENANCE OF PONDS AND ROADWAYS. 4. ALL PRIVATE TO PUBLIC STORM CONNECTIONS MUST BE INSPECTED BY
- AN ENGINEERING DEVELOPMENT REVIEW INSPECTOR. ANDSCAPE GUIDELINES: I. PAD SITES ARE TO INCORPORATE LANDSCAPED AREAS A MINIMUM OF FT. WIDE WHEN ADJACENT TO STREETS, AND IO FEET WIDE WHEN
- ADJACENT TO OTHER LANDSCAPED AREAS. 2. PLANT PALETTE SHALL BE DOMINATED BY THE FOLLOWING SPECIES: A. PARKING LOT TREES SHALL CONSISTS PRIMARILY OF KENTUCKY
- COFFEE TREES. B. SHUBERT CHOKECHERRIES SHALL BE GROUPED & PLANTED IN LINEAR PATTERNS ALONG LANDSCAPE ISLAND WHERE VISIBILITY IS CRUCIAL. C. AUSTRIAN AND SOUTHWESTERN WHITE PINES ARE TO BE PLANTED IN
- LANDSCAPE AREAS WHERE SCREENING WOULD BE DESIRABLE. ENTRIES TO INDIVIDUAL PAD SITES AND THE OVERALL DEVELOPMENT ARE TO BE ACCENTED BY PLANTING BEDS CONTAINING SMALL SHRUBS ORNAMENTAL GRASSES, PERENNIALS, AND ANNUAL FLOWERS.
- . ALL LOTS ARE REQUIRED TO COMPLY WITH CITY CODE ARTICLE 4, PAR 3, LANDSCAPE STANDARDS.

AVIGATION EASEMENT NOTE

AN AVIGATION EASEMENT EFFECTING THE SUBJECT PROPERTY AND DEVELOPMENT IS THEREIN ESTABLISHED BY THE "CREEKSIDE PLAZA" SUBDIVISION PLAT. THIS EASEMENT IS SUBJECT TO THE TERMS AND CONDITIONS AS SPECIFIED IN THE INSTRUMENT RECORDED UNDER RECEPTION NO. 217069667 OF THE RECORDS OF EL PASO COUNTY, COLORADO."

2015 IFC-ACCESS & LOADING

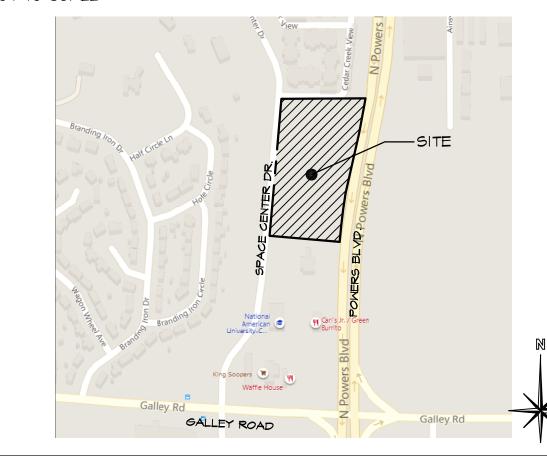
FACILITIES, BUILDINGS OR PORTIONS OF BUILDINGS HEREAFTER CONSTRUCTED SHALL BE ACCESSIBLE TO FIRE DEPARTMENT APPARATUS BY WAY OF AN APPROVED FIRE APPARATUS ACCESS ROAD WITH AN ASPHALT, CONCRETE OR OTHER APPROVED DRIVING SURFACE CAPABLE OF SUPPORTING THE IMPOSED LOAD OF FIRE APPARATUS WEIGHT AT LEAST 75,000 POUNDS WITH A MINIMUM SINGLE AXLE WEIGHT OF 27,000 POUNDS (ORDINANCE 18-50, 8.4.105, DIO2.1)

DRAWING INDEX

OF 3 - SITE PLAN & PROJECT INFORMATION, DRAWING INDEX 2 OF 3 - PRELIMINARY GRADING PLAN 3 OF 3 - PRELIMINARY UTILITY & PUBLIC FACILITIES PLAN

VICINITY MAP

NOT TO SCALE



PROJECT INFORMATION

PROPERTY INFORMATION OWNER NAME: JACKSONIA CORPORATION 107 NORTH REINO RD. STE 404 NEWBURRY PARK, CA 91320-3710 LEGAL DESCRIPTION SEE LEGAL DESCRIPTION PARCEL NUMBER 64|2|-0|-032 ZONING: PBC/CR SS APZ2 AO

CONCEPT PLAN: CREEKSIDE CENTER CONCEPT PLAN, CITY FILE NO. AR CP 99-00611-A4MN19 LOT SIZE: 223,278 SF (5.126 ACRES) CURRENT USE: VACANT FLOODPLAIN STATEMENT:. ZONE X (MAP NO: 0804100751 G. DATED DEC. 7, 2018)

GENERAL RETAIL SPACE, MEDICAL

OFFICE, RETAIL, RESTAURANT DEVELOPMENT SCHEDULE CONSTRUCTION: SUMMER 2022 LANDSCAPING:

PROPOSED SPACE PRINCIPAL USES:

DEVELOPMENT APPLICANT/DEVELOPER HAMMERS CONSTRUCTION, INC. COMPANY: 1411 WOOLSEY HEIGHTS

COLO. SPGS., CO 80915 PHONE NUMBER: (719)-570-1599FAX NUMBER:. (719)-570-7008 APPLICANT NAME: LISA PETERSON APPLICANT E-MAIL

FINANCIAL ASSURANCE NOTE

A MAXIMUM FINANCIAL CONTRIBUTION OF \$50,000.00 FOR FUTURE INTERSECTION IMPROVEMENTS AT SPACE CENTER DRIVE & GALLEY ROAD IS REQUIRED FOR CREEKSIDE CENTER. A PROPORTIONAL SHARE OF THE \$50,000., BASED ON THE ACREAGE OF THE UNDEVELOPED LAND, SHALL BE PAID BY EACH DEVELOPMENT PRIOR TO ISSUANCE OF A BUILDING PERMIT.

AMOUNTS DUE:

LOT I, CREEKSIDE CENTER FILING NO. 1 - \$ 4,390.53 LOT I, CREEKSIDE CENTER FILING NO. 2 - \$ 4,059.36 LOT I, CREEKSIDE CENTER FILING NO. 3 - \$ 11,690.19 LOT I, CREEKSIDE CENTER FILING NO. 4 - \$ 3,390.16

REMAINDER OF CREEKSIDE CENTER \$ 26,469.76 (BETWEEN FILING NO. 3 & EXISTING CREEKSIDE APARTMENTS) \$ 50,000.00

STREAMSIDE NOTE

THE PROPERTY IS PARTIALLY CONTAINED WITHIN THE STREAMSIDE OVERLAY AND IS SEPARATED FROM SAND CREEK BY SPACE CENTER DRIVE, THEREFORE, PURSUANT TO CITY CODE SECTION 7.3.508.F.3.A, THE PROPERTY IS NOT SUBJECT TO THE REGULATIONS OF THE STREAMSIDE OVERLAY ZONE.

SITE LEGEND

UTILITY EASEMENT DRAINAGE & UTILITY EASEMENT ACCESS EASEMENT - 6' HIGH WROUGHT IRON FENCE ---E ---E ---E -- ELECTRICAL LINE ---- \$ ---- SANITARY SEWER LINE STORM SEWER LINE NEW SIDEWALK LOCATIONS

PROPERTY CORNER

TRAFFIC FLOW WALL PACK LIGHTING

MANHOLE ELECTRICAL TRANSFORMER

EXISTING FIRE HYDRANT PROPOSED FIRE HYDRANT

W/ CONTROL JOINTS @ 5'-0" O.C

SITE PLAN

1 FINAL SUBMITTAL

CITY FILE NO. AR DP 21-00315

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THIS DRAWING CONTAINS MATERIAL &

CONSENT OF HAMMERS CONSTRUCTION

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FEATURES AND PLACEMENT OF T

2022 HAMMERS CONSTRUCTION

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REPRODUCED WITHOUT WRITTEN



Land Use Review Approved 09/08/2022 5:29:35 PM William.Gray

DATE: MAY 5, 2021 DRAWN BY: D. AQUINO **PROJ. MNGR**: B. GREEN SCALE: SEE PLAN APPROVED BY: JOB NO: 1100

> RESUBMITTALS: <u> 8/26/2021-COMMENTS-6/14/21</u>

 $\frac{1}{3}$ 2/23/2022-COMMENMTS-9/28/ 4\ 4/26/2022-COMMENTS-3/25/22 、7/12/2022-COMMENTS-6/30/22

